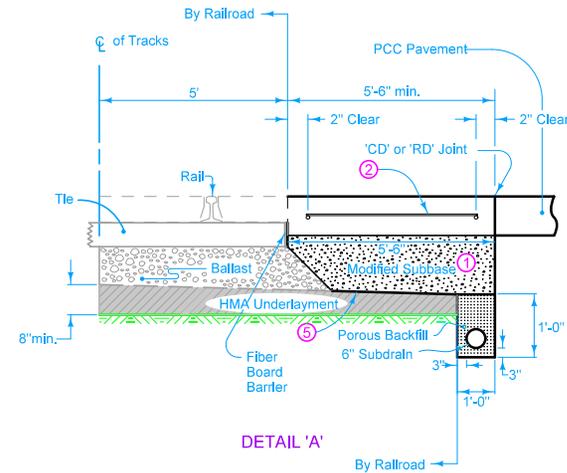
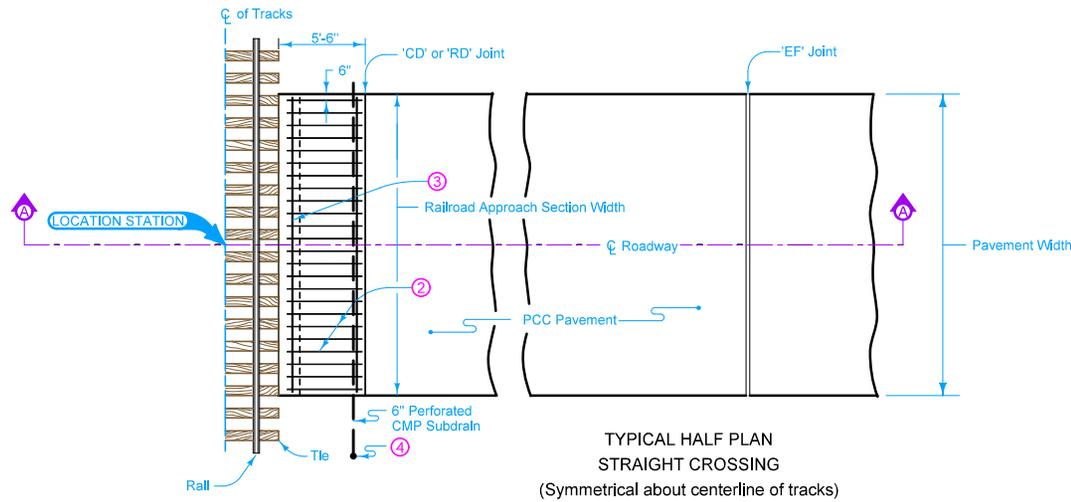


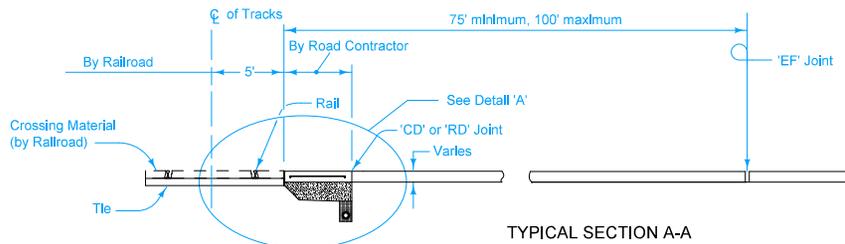
For joint details, see PV-101.

- ① Ballast meeting Railroad specifications may be substituted for modified subbase.
- ② #5 Bars at 12" centers located at half of the pavement thickness. Wire tie at all intersections with other bars. Lap a minimum of 1 foot when necessary and securely wire tie.
- ③ #5 Bars x (Approach Width - 4").
- ④ Outlet subdrain into ditch or storm sewer. See DR-303 and DR-306. Slope subdrain to drain.
- ⑤ Slope according to AREMA specifications



Possible Contract Item:  
Railroad Approach Section, P.C.C.

Possible Tabulation:  
112-3

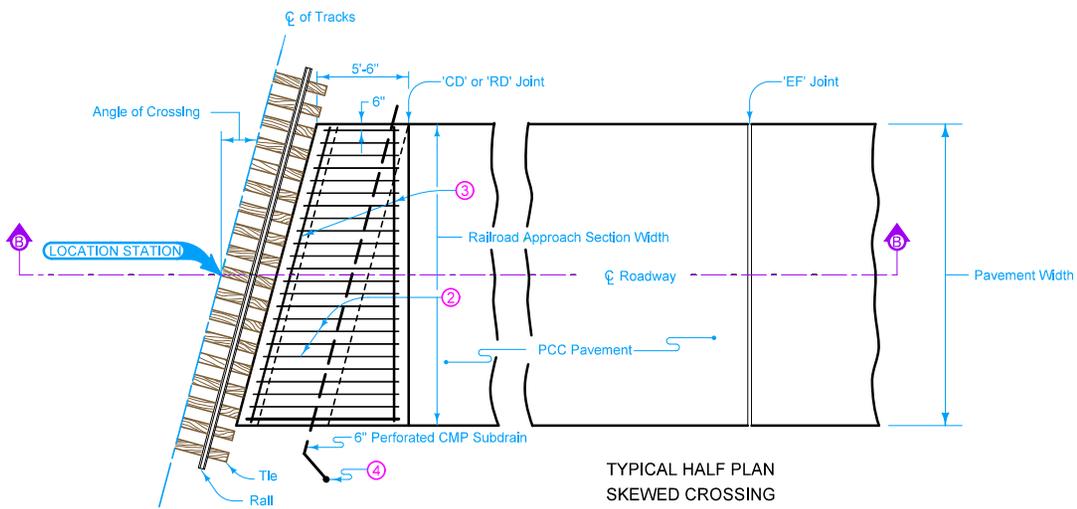


<b>IOWA DOT</b>	REVISION	
	2	10-17-17
<b>STANDARD ROAD PLAN</b>		<b>PV-106</b>
		SHEET 1 of 2

REVISIONS: Changed DR-304 to DR-306.

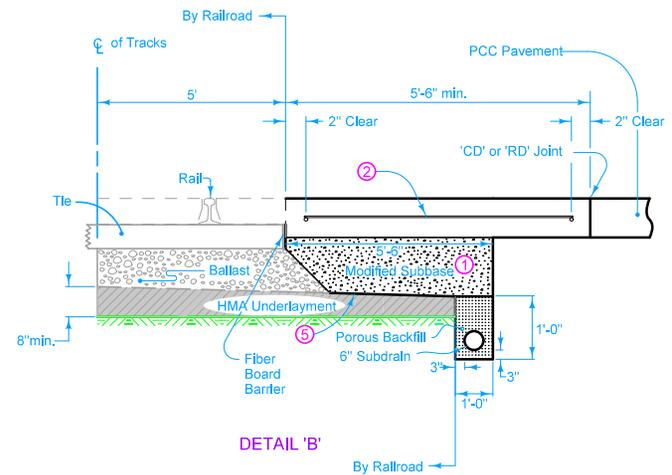
APPROVED BY DESIGN METHODS ENGINEER

PCC RAILROAD  
APPROACH SECTION

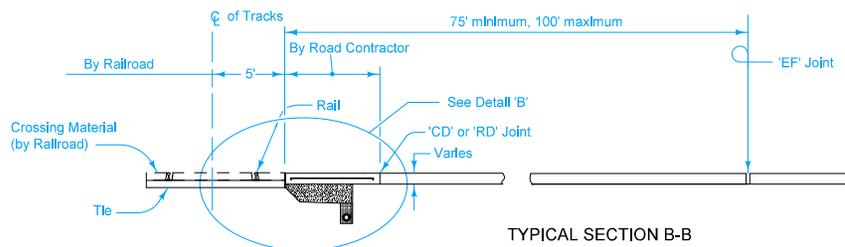


TYPICAL HALF PLAN  
SKEWED CROSSING  
(Symmetrical about centerline of tracks)

- ① Ballast meeting Railroad specifications may be substituted for modified subbase.
- ② #5 Bars at 12" centers located at half of the pavement thickness. Wire tie at all intersections with other bars. Lap a minimum of 1 foot when necessary and securely wire tie.
- ③ #5 Bars x (Approach Width - 4").
- ④ Outlet subdrain into ditch or storm sewer. See DR-303 and DR-306. Slope subdrain to drain.
- ⑤ Slope according to AREMA specifications



DETAIL 'B'



TYPICAL SECTION B-B

<b>IOWA DOT</b>	REVISION	
	2	10-17-17
<b>STANDARD ROAD PLAN</b>		<b>PV-106</b>
REVISIONS: Changed DR-304 to DR-306.		SHEET 2 of 2

*Brian Smith*  
APPROVED BY DESIGN METHODS ENGINEER

**PCC RAILROAD  
APPROACH SECTION**