

All Transverse Bars are #5.

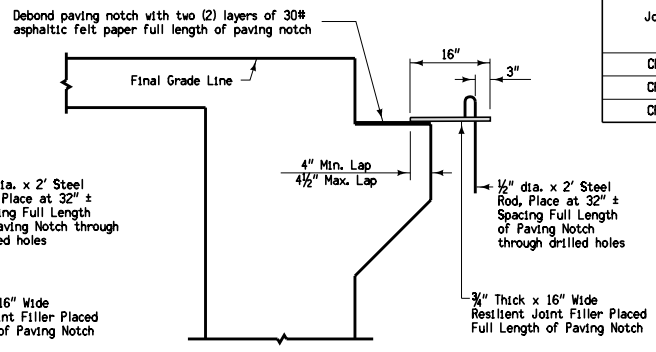
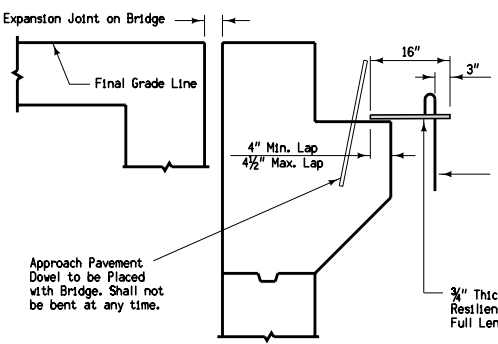
JOINT TYPE FOR MOVEABLE ABUTMENT BRIDGES

Joint	Maximum Bridge Length	
	Concrete Beam or Slab	Steel Girder
CF-1	370'	250'
CF-2	465'	320'
CF-3	575'	400'

Contract Item:
Bridge Approach, RK-20
Tabulation: 112-6

For joint details, see Standard Road Plans RH-50, RH-51, and RH-52.

- ① 2" min. to 2 1/2" max. clear to bent bar.
- ② Minimum lap length: #5 Bars - 18"
#6 Bars - 27"
#8 Bars - 48"
- ③ If bridge is skewed, place additional #5 bar parallel to skewed face.



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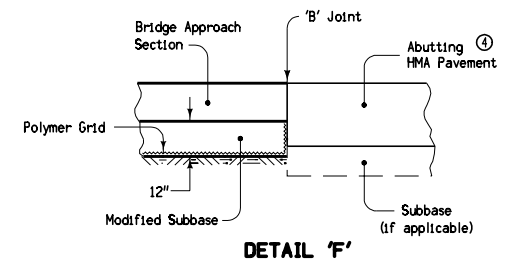
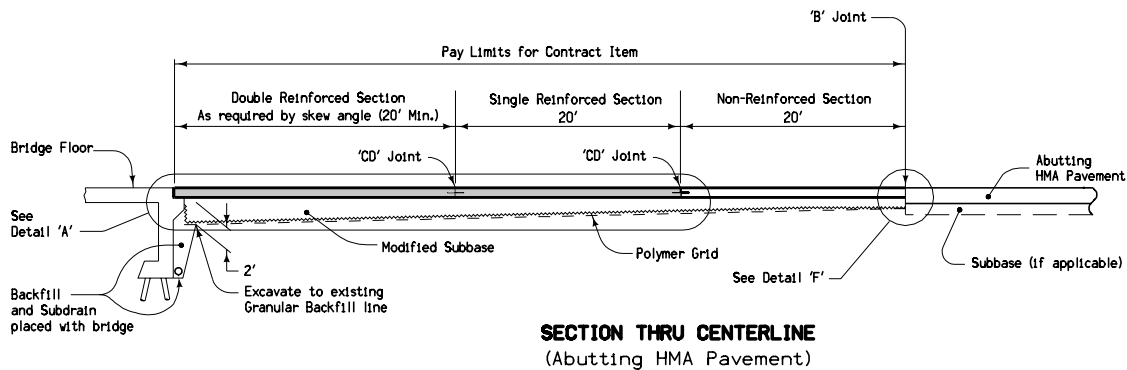
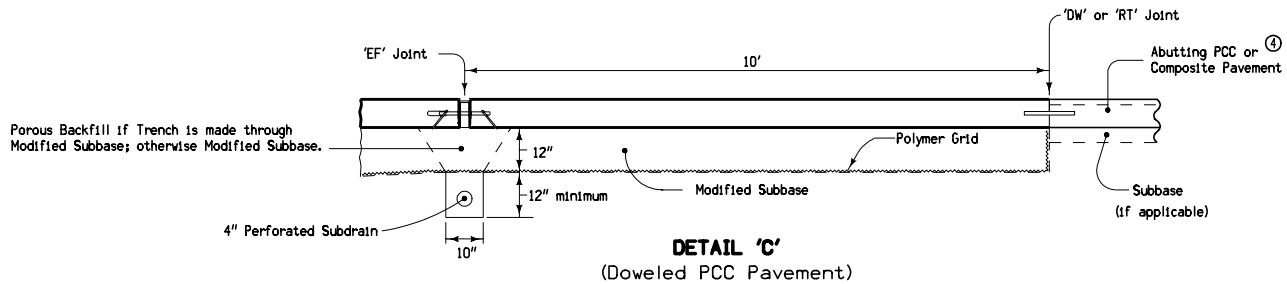
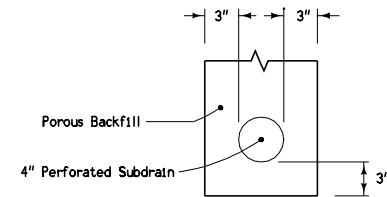
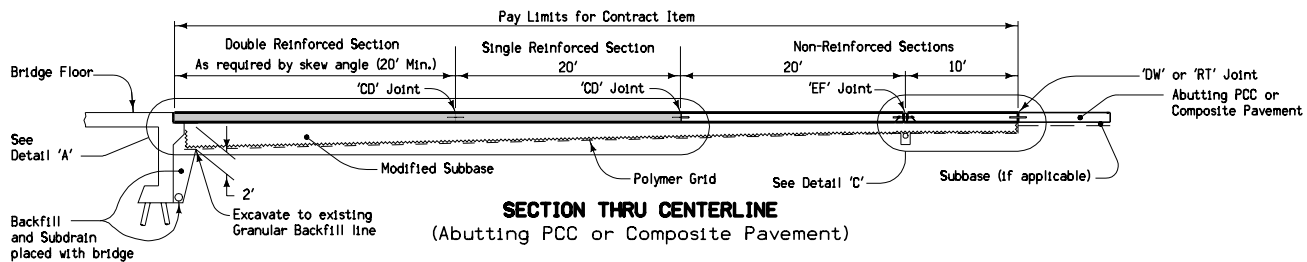
STANDARD ROAD PLAN

REVISIONS: Moved top longitudinal bar above transverse bars. Added label for centerline Approach Roadway.

Deanna Macfild
APPROVED BY DESIGN METHODS ENGINEER

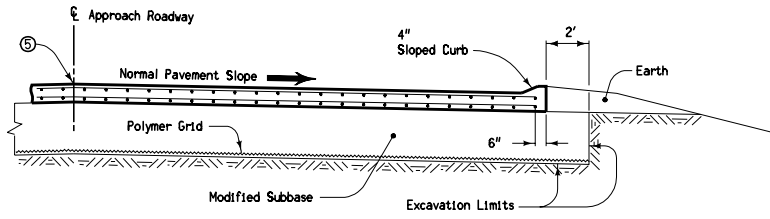
REVISION	3	10-21-08
RK-20		
SHEET 1 of 3		

DOUBLE REINFORCED 12" APPROACH

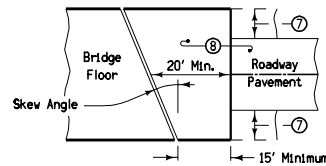


④ If abutting pavement (PCC or HMA) is not in place, see Standard Road Plan RK-30.

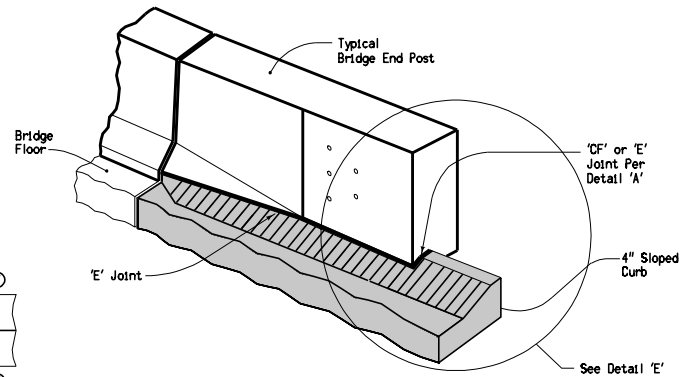
<p>Iowa Department of Transportation</p> <p>STANDARD ROAD PLAN</p> <p>REVISIONS: Moved top longitudinal bar above transverse bars. Added label for centerline Approach Roadway.</p> <p><i>Deanna Macfild</i> APPROVED BY DESIGN METHODS ENGINEER</p>	<p>REVISION</p> <p>3 10-21-08</p>
	<p>RK-20</p> <p>SHEET 2 of 3</p>
<p>DOUBLE REINFORCED 12" APPROACH</p>	



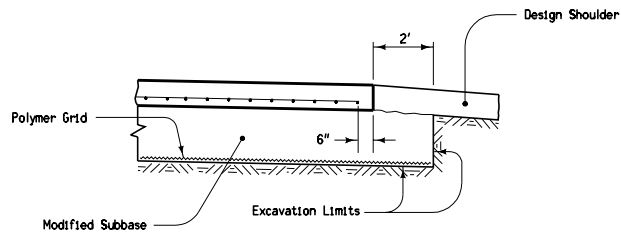
SECTION A-A ⑤



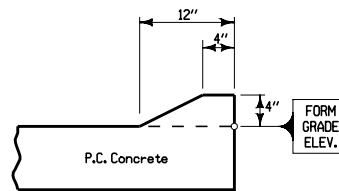
APPROACH PAVEMENT LAYOUT AT A SKEW



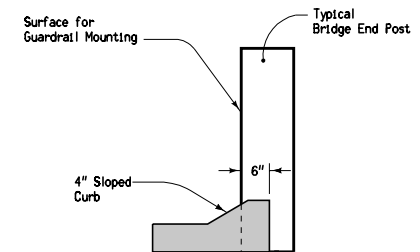
DETAIL 'D' ⑥
(Joint Placement)



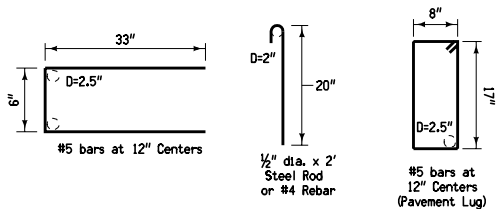
SECTION B-B ⑥



4" SLOPED CURB



DETAIL 'E'
(Back of Curb Placement)



BENT BAR SHAPES

- ⑤ Longitudinal Joint: (Standard Road Plan RH-51)
Single pour — Saw cut joint per Detail B.
Two pours — Use 'KS-2' Joint
- ⑥ See Standard Road Plan RK-21, RK-22, or RK-23.
- ⑦ Design shoulder width.
- ⑧ Reinforced bridge approach section.

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	<p>RK-20</p> <p>SHEET 3 of 3</p>
<p>DOUBLE REINFORCED 12" APPROACH</p>	