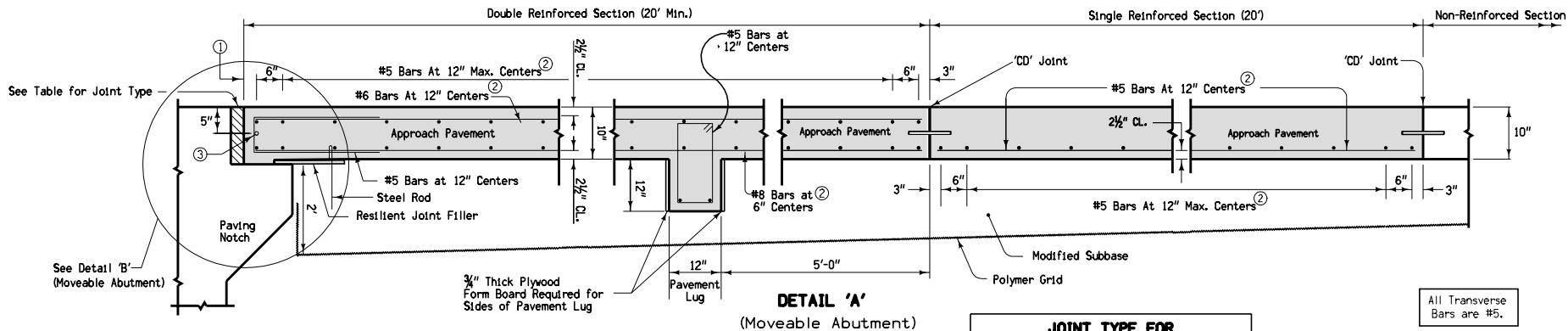


DETAIL 'A'
(Fixed Abutment)



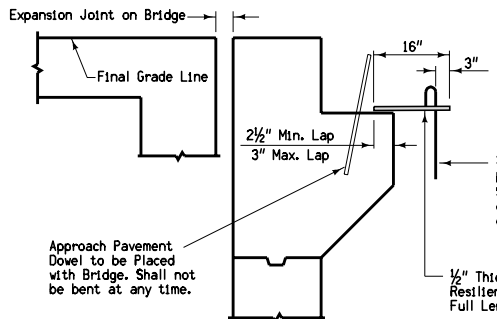
DETAIL 'A'
(Moveable Abutment)

All Transverse Bars are #5.

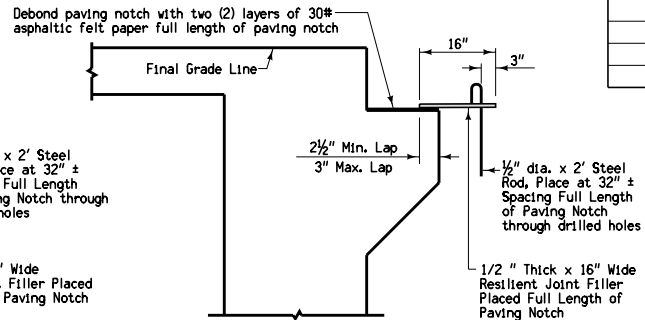
JOINT TYPE FOR MOVEABLE ABUTMENT BRIDGES

Joint	Maximum Bridge Length	
	Concrete Beam or Slab	Steel Girder
CF-1	370'	250'
CF-2	465'	320'
CF-3	575'	400'

Contract Items:
Bridge Approach, RK-25
Tabulation: 112-6



DETAIL 'B' (Fixed Abutment)



DETAIL 'B' (Moveable Abutment)

For joint details, see Standard Road Plans RH-50, RH-51, and RH-52.

- ① 2" min. to 2 1/2" max. clear to bent bar.
- ② Minimum lap length: #5 Bars - 18"
#6 Bars - 27"
#8 Bars - 48"
- ③ If bridge is skewed, place additional #5 bar parallel to skewed face.

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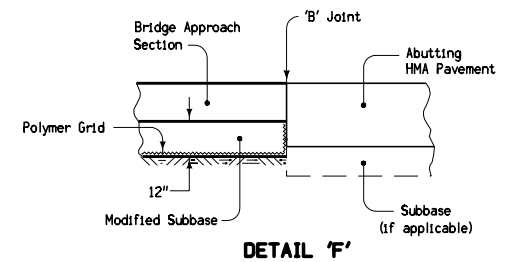
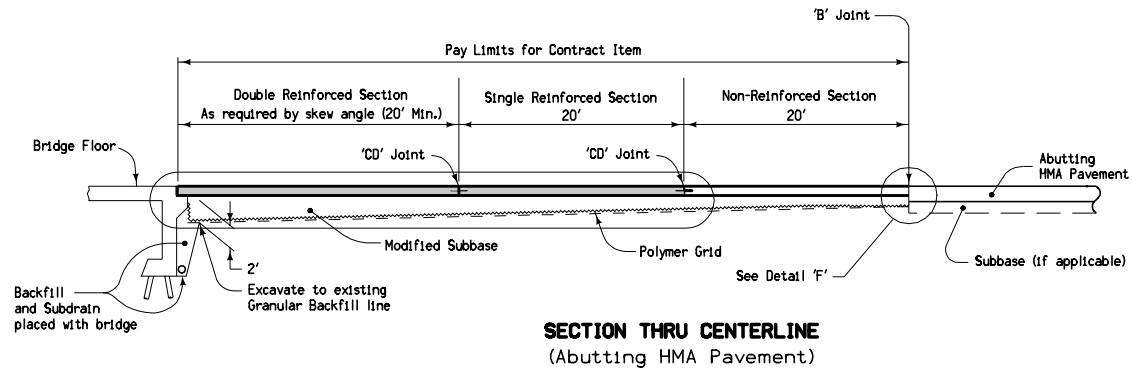
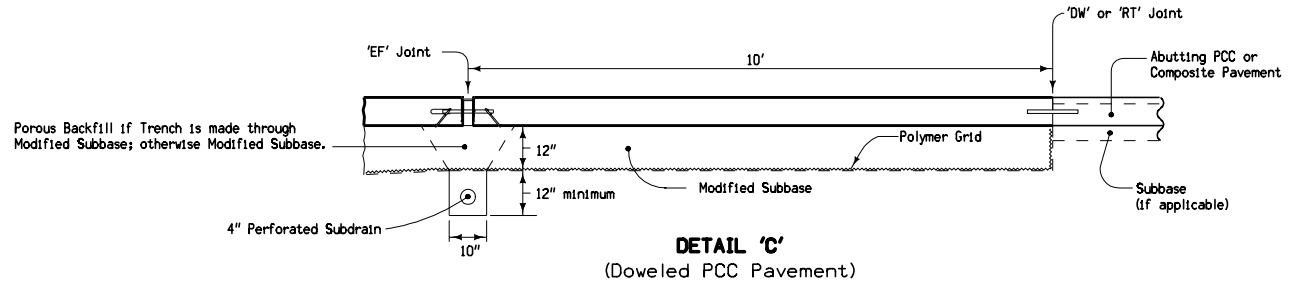
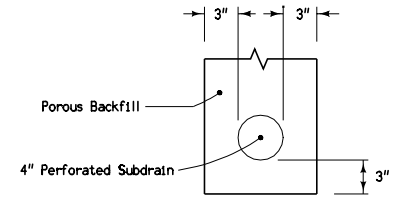
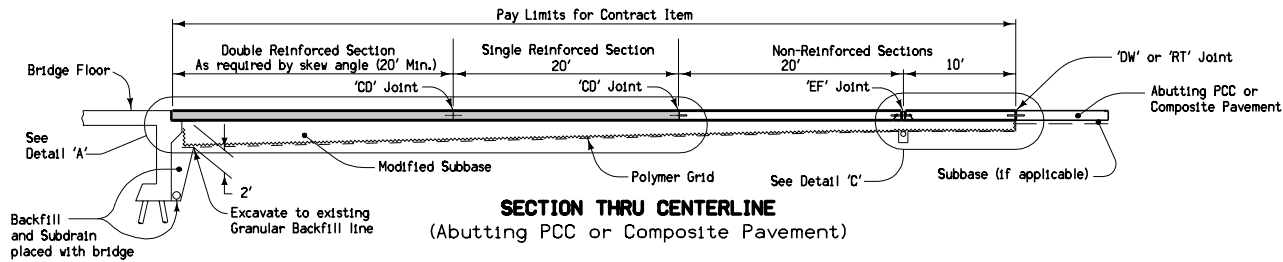
STANDARD ROAD PLAN

REVISIONS: Moved top longitudinal bar above transverse bars. Added table for centerline Approach Roadway.

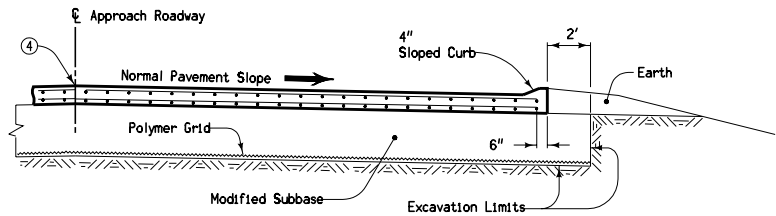
Deanna Macfild
APPROVED BY DESIGN METHODS ENGINEER

REVISION	3	10-21-08
RK-25		SHEET 1 of 3

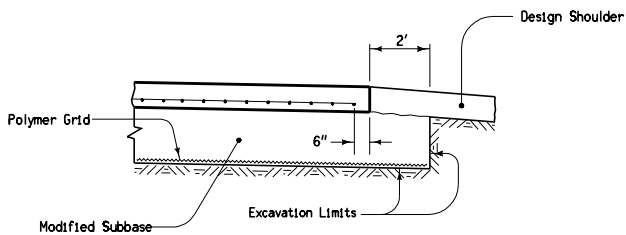
DOUBLE REINFORCED 10" APPROACH



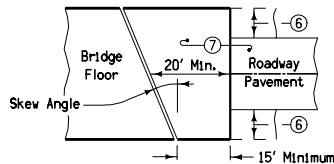
<p>Iowa Department of Transportation</p> <p>STANDARD ROAD PLAN</p> <p>REVISIONS: Moved top longitudinal bar above transverse bars. Added table for centerline Approach Roadway.</p> <p><i>Deanna Macfild</i> APPROVED BY DESIGN METHODS ENGINEER</p>	<p>REVISION</p> <p>3 10-21-08</p>
	<p>RK-25</p> <p>SHEET 2 of 3</p>
<p>DOUBLE REINFORCED 10" APPROACH</p>	



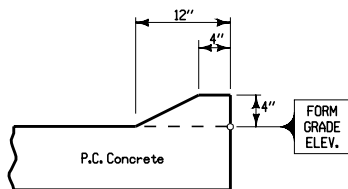
SECTION A-A ⑤



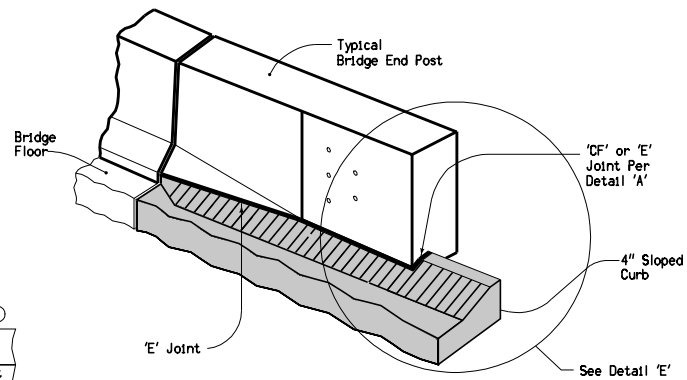
SECTION B-B ⑤



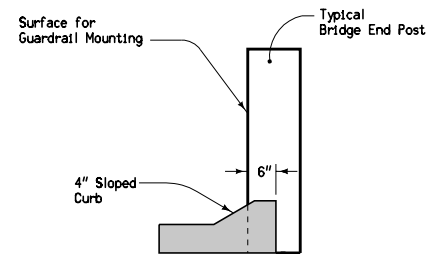
APPROACH PAVEMENT LAYOUT AT A SKEW



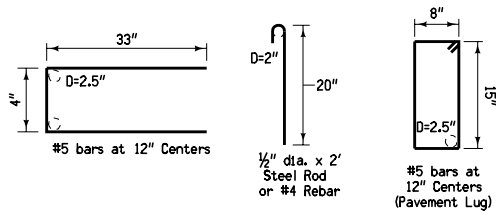
4" SLOPED CURB



DETAIL 'D' ⑤
(Joint Placement)



DETAIL 'E'
(Back of Curb Placement)



BENT BAR SHAPES

④ Longitudinal Joint: (Standard Road Plan RH-51)
Single pour — Saw cut joint per Detail B.
Two pours — Use 'KS-2' Joint

⑤ See Standard Road Plan RK-21, RK-22, or RK-23.

⑥ Design shoulder width.

⑦ Reinforced bridge approach section.

 Iowa Department of Transportation	REVISION	
	3	10-21-08
STANDARD ROAD PLAN	RK-25	
	SHEET 3 of 3	

REVISIONS: Moved top longitudinal bar above transverse bars.
Added table for centerline Approach Roadway.

Deanna Macfild
APPROVED BY DESIGN METHODS ENGINEER

DOUBLE REINFORCED 10" APPROACH