

Loop exit pavement shall be the same thickness as mainline pavement.

Loop exit pavement shown by shaded area is 1743 square yards.

Special shaping of area between lines A and B may be required to assure proper drainage.

For joining layout, see Standard Road Plan RV-11.

This design is based on 60 mph design speed at e max = 6%.

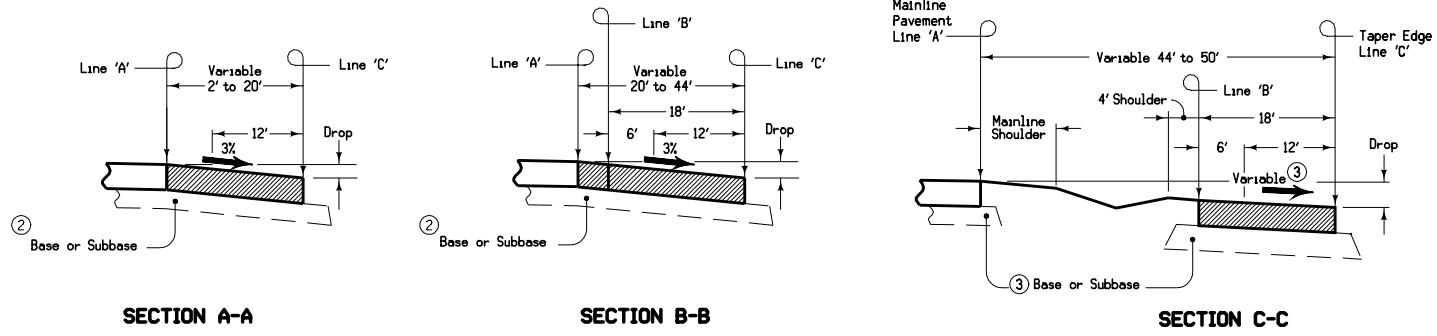
- ① For header construction details at the beginning of taper, refer to Typical 7101 or Typical 7102.
- ② Subbase for loop exit pavement shall be the same thickness as mainline subbase.
- ③ The loop pavement cross slope between (K) and (M) is determined by superelevation rotated about line C. Refer to Standard Road Plan RP-3 and plans for superelevation transition requirements.

W ₀	Shoulder Width beyond Edge of Mainline Pavement		
	8'	10'	12'
12'	NA	60'	90'
14'	30'	60'	NA

NOTE: W₀ is the width of the outside lane to the Edge of Pavement.

DISTANCE (Ft.)	750	725	700	675	650	625	600	575	550	525	500	475	450	425	400	375	350	325	300	275	270	250	225	200	175	150	125	100	75	50	25	0
OFFSET (Ft.)	0.00	1.67	3.33	5.00	8.33	10.00	11.67	13.33	15.00	16.67	18.00	18.33	20.00	21.67	23.33	25.00	26.67	28.33	30.00	31.67	33.33	35.00	36.67	38.33	40.00	41.67	43.33	44.00	45.00	46.67	48.33	50.00
DROP (Ft.)	0.00	0.05	0.10	0.15	0.25	0.30	0.35	0.40	0.45	0.50	0.54	0.55	0.60	0.65	0.70	0.75	0.80	0.85	0.90	0.95	1.00	1.05	1.10	1.15	1.20	1.25	1.30	1.32	1.35	1.40	1.45	1.50

NOTE: The elevations at edge of taper from BEGIN TAPER to POINT 'M' are established by a constant 3% slope across the appropriate taper widths based on the Taper Ratio of 15:1, Drop = (0.03) x (Offset).



 Iowa Department of Transportation	REVISION
	5 10-21-08
STANDARD ROAD PLAN	RV-8
SHEET 1 of 1	
REVISIONS: Corrected circle note 2 and table title. Removed mainline pavement and shoulder widths. Added table for shoulder transition distance.	
<i>Deanna Masfeld</i> APPROVED BY DESIGN METHODS ENGINEER	
DECELERATION TAPER FOR 18' EXIT LOOP	