



Iowa Department of Transportation

MINUTES OF IOWA DOT SPECIFICATION COMMITTEE MEETING

October 13 2005

Members Present:	Tom Reis, Chair Daniel Harness, Secretary Keith Norris Gary Novey John Smythe Larry Jesse Jim Berger Doug McDonald	Specifications Section Specifications Section District 2-Dist. Mat. Engineer Office of Bridges & Structures Office of Construction Office of Local Systems Office of Materials District 1 - Marshalltown RCE Office
Members Not Present:	John Adam Mike Kennerly Bruce Kuehl Roger Bierbaum Troy Jerman	Statewide Operations Bureau Office of Design District 6-Dist. Const. Engineer Office of Contracts Office of Traffic & Safety
Advisory Members Present:	Lisa Rold	FHWA
Advisory Members Not Present:	Jim Rost Larry Stevens	Office of Location & Environment SUDAS
Others Present:	Mark Bortle Deanna Maifield Ed Kasper Dan Sprengeler	Office of Construction Office of Design Office of Contracts Office of Traffic & Safety

Tom Reis, Specifications Engineer, opened the meeting. The following items were discussed in accordance with the October 6, 2005 agenda, except the Office of Construction requested Item 3 be withdrawn:

1. Article 1102.17, Disadvantaged Business Enterprises.

The Office of Contracts requested changes to Article 1102.17 that will modify the DBE specifications concerning how contractors are credited with utilizing DBE firms.

2. Article 2107.02 A, Compaction Equipment (Embankments).

The Office of Construction requested a change to Article 2107.02 that will clarify the compaction equipment requirements for embankments constructed of granular materials.

3. Article 2303.02, Paver (Hot Mix Asphalt Mixtures).

Withdrawn.

4. Section 2528, Traffic Control.

The Office of Construction requested several changes to Section 2528 that will update the worker attire, update flagger attire to meet the 2003 MUTCD, and development of a Developmental Specification for Night Work Lighting. This item had been deferred from the September 8, 2005 Specification Committee meeting.

5. Developmental Specification for Night Work Lighting.

The Office of Construction requested a discussion concerning the use of a new Developmental Specification for Night Work Lighting.

6. Standard Specification Manual Rewrite.

This item was a general discussion on the best approach to implement section of the Standard Specification manual as they are rewritten.

SPECIFICATION REVISION SUBMITTAL FORM

Submitted by: Roger Bierbaum		Office: Contracts	Item 1
Submittal Date: September 7, 2005		Proposed Effective Date: April 2006 GS	
Article No.: 1102.17, D, 3, c. Title: Contractors with History of Utilizing DBEs		Other:	
Specification Committee Action:			
Deferred:	Not Approved:	Approved Date: 10/13/05	Effective Date: 4/18/06
Specification Committee Approved Text: See Specification Section Recommended text.			
<p>Comments: The Office of Contracts explained that the change removes one factor used to calculate a contractor's good faith effort points (or "goodie points"). Goodie points are used as one method to determine responsiveness to the DBE goal at the time of award. The low bidder has to be responsive to meet the DBE goal. Taking the X factor out will change how goodie points are calculated. The intent is to encourage contractors to spread work around to various DBEs rather than the same DBE. Contractors will no longer receive goodie points if they use only one DBE for more than 50% of the DBE work. The Specification Section noted that this change is in the Administrative Rules section and will need to be presented to the Legislature. This process typically takes approximately six months. Approval for the April 2006 letting may be possible.</p>			
Specification Section Recommended Text:			
1102.17, D, 3, c., Contractors with History of Utilizing DBEs			
Delete sub-Article 3) and 4):			
<p>3) A range of minus 2.0 points to plus 2.0 points for usage of multiple DBE firms using the following formula:</p>			
<p>$X = \frac{\text{No. DBE firms used}}{\text{No. DBE subcontracts}}$ $X = \frac{\text{No. Subcontractors used}}{\text{No. Subcontracts}}$</p>			
		X	Points
		<.80	-2.0
		.80-.84	-1.5
		.85-.89	-1.0
		.90-.94	-0.5
		.95-1.05	0.0
		1.06-1.10	0.5
		1.11-1.15	1.0
		1.16-1.20	1.5
		>1.20	2.0
<p>4) Up to 4.0 points for participating in DBE assistance programs (e.g. formal mentor protégé programs, big brother/sister programs, or other programs to assist DBEs) in which the Contractor is currently involved. Participation in a formal mentor protégé program would be required to earn more than 2.0 points.</p>			

Add as the second sentence of the third paragraph:
 A contractor under consideration for having a history of utilizing DBE firms must have been awarded at least two contracts during the period being reviewed. **Contractors who have used the same DBE firm for over 50% of their subcontract dollars with DBE firms will not be considered as having a history of utilizing DBEs.**

Comments:

Member's Requested Change: (DO NOT USE "Track Changes," or "Mark-Up". Use Strikeout/Highlight)

Delete the calculations 3) and 4) under 1102.17 D.3.c. Contractors with History of Utilizing DBEs. Also, add a new sentence at the end of first paragraph after the deleted text.

3) A range of minus 2.0 points to plus 2.0 points for usage of multiple DBE firms using the following formula:

$$X = \frac{\text{No. DBE firms used} / \text{No. DBE subcontracts}}{\text{No. Subcontractors used} / \text{No. Subcontracts}}$$

X	Points
<.80	-2.0
.80-.84	-1.5
.85-.89	-1.0
.90-.94	-0.5
.95-1.05	0.0
1.06-1.10	0.5
1.11-1.15	1.0
1.16-1.20	1.5
>1.20	2.0

4) Up to 4.0 points for participating in DBE assistance programs (e.g. formal mentor protégé programs, big brother/sister programs, or other programs to assist DBEs) in which the Contractor is currently involved. Participation in a formal mentor protégé program would be required to earn more than 2.0 points.

A contractor under consideration for having a history of utilizing DBE firms must have been awarded at least two contracts during the period being reviewed. **Contractors who have used the same DBE firm for over 50% of their subcontract dollars with DBE firms will not be considered as having a history of utilizing DBEs.**

Reason for Revision: Contractors could gain 6 points or lose 2 points based on the X factor. This number of points was not a major impact when the annual DBE goal was 10%, but becomes a major impact with an annual DBE goal of 5.0%. Also any overuse of the same DBEs firms by a contractor will be addressed by other means.

County or City Input Needed (X one)	Yes	No X
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Comments:

Industry Input Needed (X one)	Yes	No X
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Industry Notified:	Yes X	No	Industry Concurrence:	Yes X	No
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Comments: Requested through the AGC/DBE/IDOT Task Force Meeting

SPECIFICATION REVISION SUBMITTAL FORM

Submitted by: John Smythe		Office: Construction		Item 2	
Submittal Date: September 07, 2005			Proposed Effective Date: April 2006 GS		
Article No.: 2107.02, A Title: Compaction Equipment (Embankments)			Other:		
Specification Committee Action:					
Deferred:		Not Approved:		Approved Date: 10/13/05	Effective Date: 4/18/06
Specification Committee Approved Text: See Specification Committee Recommended Text.					
Comments: Office of Construction explained that the new language brings this specification into conformance with the current state of practice.					
Specification Section Recommended Text:					
2107.02, A, Compaction Equipment.					
Replace the second paragraph:					
<p>On embankments constructed primarily of sand or other granular material which will not readily compact with a sheepsfoot type roller, the Contractor may, with the Engineer's approval, substitute a pneumatic tired roller weighing not less than 200 pounds per inch (3.5 kg/mm) width of roller in lieu of the sheepsfoot type roller. For compaction of sand or other granular material, the Contractor shall use a self propelled, pneumatic roller meeting the requirements of Article 2001.05, C, or a self propelled vibratory roller meeting the requirements of Article 2001.05, F.</p>					
Comments:					
Member's Requested Change: (DO NOT USE "Track Changes," or "Mark-Up". Use Strikeout Highlight)					
Delete the second paragraph in Item A, "Compaction Equipment" and replace it with the new paragraph:					
<p>On embankments constructed primarily of sand or other granular material which will not readily compact with a sheepsfoot type roller, the Contractor may, with the Engineer's approval, substitute a pneumatic tired roller weighing not less than 200 pounds per inch (3.5 kg/mm) width of roller in lieu of the sheepsfoot type roller.</p>					
For compaction of sand or other granular material, the Contractor shall use a self propelled, pneumatic roller meeting the requirements of Article 2001.05 C or a self propelled vibratory roller meeting the requirements of Articles 2001.05 F.					

Reason for Revision: This is a recommendation from the FHWA after the process review.					
County or City Input Needed (X one)			Yes	No X	
Comments:					
Industry Input Needed (X one)			Yes	No X	
Industry Notified:	Yes	No X	Industry Concurrence:	Yes	No
Comments:					

SPECIFICATION REVISION SUBMITTAL FORM

Submitted by: John Smythe		Office: Construction		Item 3	
Submittal Date: September 28, 2005		Proposed Effective Date: April 2006 GS			
Article No.: 2303.02, F, 2, Title: Paver (Hot Mix Asphalt Mixtures)		Other:			
Specification Committee Action:					
Deferred: X		Not Approved:		Approved Date:	
Effective Date:					
Specification Committee Approved Text:					
Comments: Deferred at the request of the Office of Construction.					
Specification Section Recommended Text:					
<p>Article 2303.02, F, 2, Paver</p> <p>Add as the second sentence:</p> <p>Article 2001.19 shall apply. A tracked paver, equipped with crawler treads, shall be used to place the first lift of HMA over subgrade or subbase. Spreaders, as described in Article 2001.13, D, may be used to place paved shoulders. Spreaders used to place the final lift of paved shoulders shall meet additional requirements of Article 2001.19.</p>					
Comments:					
Member's Requested Change (DO NOT USE "TRACK CHANGES," use Strikeout/Highlight):					
<p>Insert the following highlighted text as a new second sentence of the paragraph for the second numbered item (2. Paver) under F. Equipment:</p> <p>A tracked paver, equipped with crawler treads, shall be used to place the first lift of HMA over subgrade or subbase.</p> <p>Also, move the existing second and third sentences of the above paragraph to become a new second paragraph under 2. Paver.</p>					

Reason for Revision: To address problems associated with the use of asphalt pavers equipped with pneumatic tires on the first lift of full-depth HMA paving projects. The current specification allows the use of either type of paver in this situation.					
County or City Input Needed (X one)			Yes	No X	
Comments:					
Industry Input Needed (X one)			Yes X	No	
Industry Notified:	Yes X	No	Industry Concurrence:	Yes	No
Comments: Industry has been informed of proposed specification revision. We are awaiting a response.					

SPECIFICATION REVISION SUBMITTAL FORM

Submitted by: John Smythe		Office: Construction	Item 4
Submittal Date: October 6, 2005		Proposed Effective Date: April 2006 GS	
Article No.: 2528.01 Title: Description Article No.: 2528.10 Title: Flaggers Article No.: 2528.11 Title: Limitations		Other:	
Specification Committee Action:			
Deferred:	Not Approved:	Approved Date: 10/13/05	Effective Date: 4/18/06
Specification Committee Approved Text:			
<p>For Articles 2528.01 and 2528.10, see the Specification Section Recommended Text.</p> <p>2528.11 Limitations.</p> <p>Replace the fourth and fifth paragraphs:</p> <p>During daylight hours, workers exposed to traffic in or adjacent to traffic lanes, should wear a vest, shirt, or jacket equal to that required for flaggers according to Article 2528.10.</p> <p>At night, workers shall wear clothing that is similar in color to that required for flaggers and is retroreflective to be highly visible to drivers. The retroreflective clothing shall be designed to identify clearly the wearer as a person and shall be visible through the full ranges of body actions.</p> <p>At night, workers shall wear ANSI 107 Class 2 apparel if they are within 15 feet (4.6 m) of an open lane of traffic, unless they are shielded by temporary barrier rail or inside a vehicle cab.</p>			
<p>Comments: The Office of Construction explained that they worked with the Office of Traffic and Safety, the Office of Design, and the Specifications Section to work out the language for the last sentence in Article 2528.11. The 15 foot distance was determined from the clear zone for a work area.</p> <p>The Office of Construction also discussed the upcoming changes to the Flagger Manual. Several references are being made to the same ANSI 107 standards referenced in the proposed language for Article 2528.11. Use of the SLOW/SLOW paddle is being discontinued. References to retroreflectorized materials have been deleted since these are covered in ANSI. On page 13 of the Flagger Manual are instruction for use of the SLOW/SLOW paddle. The Office of Construction is proposing to eliminate that language.</p> <p>The Specifications Section asked if any of the Standard Road Plans would be affected. The Office of Construction explained that some of the notes on RS-63A and B as well as RS-64A and B would be affected. The Office of Design noted that these changes would be ready for the April letting.</p>			

Specification Section Recommended Text:

2528.01 Description.

Delete the third sentence of the eighth paragraph:

Gender specific signs, such as FLAGMAN and MEN WORKING, will not be allowed. The signs shall either be neutral gender, as FLAGGER, or equivalent symbol signs. ~~Control of traffic through work areas with flaggers shall also conform with the Iowa Flagger's Handbook, available from the Engineer.~~

2528.10 Flaggers.

Replace the fourth and fifth paragraphs:

~~Except in an emergency, flaggers shall use signs as specified in the current edition of the MUTCD, Part VI, except the signs shall be at least 24 inches (600 mm) wide. The sign shall be mounted on a staff with a clear distance of 6 feet (1.8 m) above the road surface.~~

~~To be visible to traffic while flagging, the flagger shall wear a soft cap or a hard hat and a vest, shirt, or jacket. The colors of these articles of dress shall be orange or strong yellow-green, or fluorescent versions of these colors. Combinations of these colors are acceptable.~~

~~Flagger operations, equipment, and apparel shall conform to the Iowa DOT Flagger's Handbook.~~

2528.11 Limitations.

Replace the fourth and fifth paragraphs:

~~During daylight hours, workers exposed to traffic in or adjacent to traffic lanes, should wear a vest, shirt, or jacket equal to that required for flaggers according to [Article 2528.10](#).~~

~~At night, workers shall wear clothing that is similar in color to that required for flaggers and is retroreflective to be highly visible to drivers. The retroreflective clothing shall be designed to identify clearly the wearer as a person and shall be visible through the full ranges of body actions.~~

~~At night, workers shall wear ANSI 107Class 2 apparel if they are within 15 feet of an open lane of traffic, unless they are shielded by temporary barrier rail or inside a vehicle cab.~~

Comments:

Member's Requested Change: (DO NOT USE "Track Changes," or "Mark-Up". Use ~~Strikeout~~/Highlight)

2528.01 DESCRIPTION, delete the last sentence of paragraph 8

Gender specific signs, such as FLAGMAN and MEN WORKING, will not be allowed. The signs shall either be neutral gender, as FLAGGER, or equivalent symbol signs. ~~Control of traffic through work areas with flaggers shall also conform with the Iowa Flagger's Handbook, available from the Engineer.~~

2528.10 FLAGGERS, delete paragraphs 4 and 5 and add a new paragraph 4

The flaggers shall be trained about safe flagging operations that comply with Iowa DOT Flagger Handbook, Part VI of the MUTCD, and the Standard Specifications prior to flagging operations. Training of flaggers shall include the following:

1. Issue and review the current Iowa DOT Flagger Handbook,
2. Presentation of the current Iowa Professional Flagging Video,
3. Issue flagger training card, which shall include the following:

- Employee name,
- Date of training,
- Name of Instructor,
- Expiration date of December 31 of the year following the training date.

The flaggers shall carry their flagger training card at all times and show it upon request.

4. Contractor shall maintain a list of the flaggers trained and the date of the training.

Training shall not be required for short time, emergency, or relief assignment of employees to flagging operations. Payment will not be made in accordance with [Article 2528.12, A, 7](#).

~~Except in an emergency, flaggers shall use signs as specified in the current edition of the MUTCD, Part VI, except the signs shall be at least 24 inches (600 mm) wide. The sign shall be mounted on a staff with a clear distance of 6 feet (1.8 m) above the road surface.~~

~~To be visible to traffic while flagging, the flagger shall wear a soft cap or a hard hat and a vest, shirt, or jacket. The colors of these articles of dress shall be orange or strong yellow-green, or fluorescent versions of these colors. Combinations of these colors are acceptable.~~

~~Flagger operations, flagger equipment, and flagger apparel shall conform to the Iowa DOT Flagger's Handbook.~~

2528.11 LIMITATIONS, delete paragraphs 4 and 5 and add a new paragraph 4

~~During daylight hours, workers exposed to traffic in or adjacent to traffic lanes, should wear a vest, shirt, or jacket equal to that required for flaggers according to [Article 2528.10](#).~~

~~At night, workers shall wear clothing that is similar in color to that required for flaggers and is retroreflective to be highly visible to drivers. The retroreflective clothing shall be designed to identify clearly the wearer as a person and shall be visible through the full ranges of body actions.~~

~~At night, workers shall wear ANSI 107 Class 2 apparel if they are within 15 feet of an open lane of traffic, unless they are shielded by temporary barrier rail or inside a vehicle cab.~~

Reason for Revision: The 2003 MUTCD which will be adopted for the Department by the Administrative Rules Committee on November 16, 2005 includes revised requirements for flagger and worker apparel. These proposed changes are being made to keep our specifications current. The January 2002 edition of the Flagger's Handbook will also need to be revised.					
County or City Input Needed (X one)			Yes	No X	
Comments:					
Industry Input Needed (X one)			Yes	No X	
Industry Notified:	Yes	No X	Industry Concurrence:	Yes	No X
Comments:					

FLAGGER's HANDBOOK
Page 3

Flagger Clothing and Equipment Clothing

In addition to being dressed neatly, flaggers need to be dressed for safety. In particular, flaggers are required to be easily visible to traffic. **Flagger's shall Always** wear the following required items, even when serving as a replacement for a short period (see Figure 1):

- an ANSI 107 Class 2 vest, shirt, or jacket in yellow green, orange, or fluorescent versions of these colors (combinations of these colors are acceptable) and
- a soft cap in yellow green, orange, or fluorescent versions of these colors meeting ANSI 107 headwear requirements; combinations of these colors are acceptable. (A hard hat in the same colors is an acceptable alternative to the soft cap and may be required by OSHA or your employer in certain circumstances.)

Equipment

Flaggers shall be equipped with the standard combination STOP/SLOW or SLOW/SLOW staff-mounted sign (see Figure 1). Retroreflective sheeting shall comply with applicable specifications. Optional but useful equipment and supplies include the following:

- a handheld radio for communications,
- adverse weather gear,
- pad and pencil, and
- air horn or whistle.

FLAGGER's HANDBOOK
Page 4

Note: A red flag is permitted **only** in the following situations:

- stopping traffic in combination with the STOP/SLOW sign (see page 8), or
- in emergencies where standard signs are not available (see pages 18–21 for information about emergency flagging).

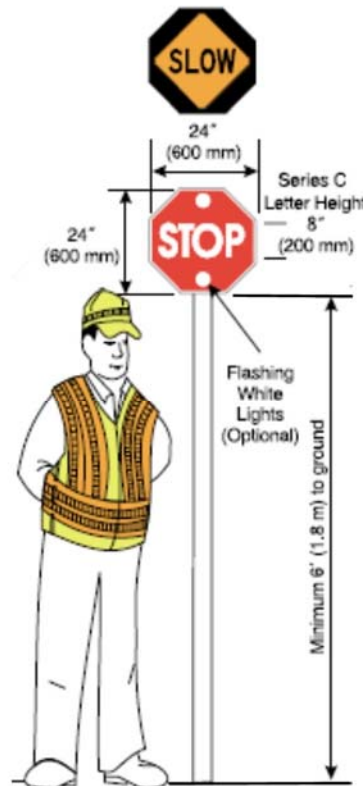


Figure 1
Clothing and equipment for daytime flagging operations

(New image)

FLAGGER's HANDBOOK
Page 15

Night Operations

When controlling traffic at night, take extra care to be visible, to guide traffic through the area, and to protect yourself from injury.

Night clothing

When flagging at night, **Flagger's shall** wear the following required items (see Figure 10):

- an ANSI 107 Class 2 retroreflectorized vest, shirt, or jacket and ANSI 107 Class E retroreflectorized pants in yellow green, or orange, or fluorescent versions of these colors (combinations of these colors are acceptable), and
- a retroreflectorized soft cap in yellow green, or orange, or fluorescent versions of these colors meeting ANSI 107 headwear requirements; combinations of these colors are acceptable. (A retroreflectorized hard hat in the same colors is an acceptable alternative to the soft cap and may be required by OSHA or your employer in certain circumstances.)

Consider wearing highly visible retroreflectorized gloves.

Note: Retroreflectorized materials in clothing and signs shall be yellow green, orange, white, silver, or fluorescent versions of these colors and shall be visible from a minimum distance of 1,000 feet (330 m). The retroreflective clothing shall be designed to clearly identify the wearer as a person.

FLAGGER's HANDBOOK
Page 16

Night equipment

At night, flaggers shall be equipped with the following (see Figure 10):

- retroreflectorized signs and other devices (retroreflective sheeting shall comply with applicable specifications),
- flashlight with nose cone, lantern, or other lighted signal that will display a red warning light, and
- a lighted flagging station (see Figure 11).

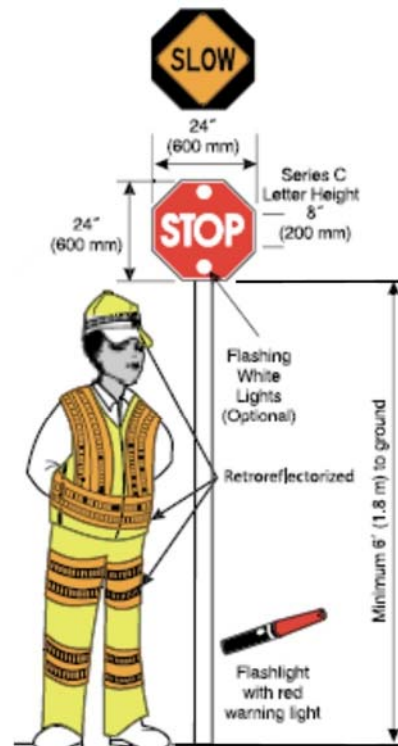


Figure 10
Clothing and equipment for nighttime flagging operations

(New image)

Developmental Specification for Night Work Lighting

Item 5

The elimination of the SLOW/SLOW paddle caused some concern for some members of the committee especially regarding night work. The Office of Construction proposed this DS to increase drivers' ability to see workers working near an open traffic lane. Special Provisions have been used in the past for night work lighting. Some of the language in this proposed DS comes from those SPs. In addition, the Office of Construction has researched what other states are doing. Some of the language comes from that research.

The Specifications Section asked if conspicuity markings would be required as was stated in the original proposal. The Office of Construction noted that this was eliminated so that Federal Motor Carrier guides would not be superseded. The Specifications Section asked if it is possible eliminate the drawings of the signs from the DS by having contractors reference appropriate materials. The Office of Contracts wasn't sure if this is possible, but they noted they would be comfortable referencing a manual if it contains these signs.

District 2 asked if a contractor chooses to do night work, but it is not required, could this DS be enforced? The Office of Construction explained that this DS would be part of allowing the contractor to do night work.

The Office of Construction asked if this DS could be implemented before April. The Specifications Section noted that it could be applied to the December letting. The Committee decided that this DS won't need a controller since its use would be determined by the concept and by the traffic control notes. The Specifications Section noted that they will continue to prepare this DS for the December letting.

DS-XXXXX
(New)



Iowa Department of Transportation

DEVELOPMENTAL SPECIFICATIONS FOR NIGHT WORK LIGHTING

Effective Date
XXXXX XX, XXXX

THE STANDARD SPECIFICATIONS, SERIES 2001, ARE AMENDED BY THE FOLLOWING MODIFICATIONS AND ADDITIONS. THESE ARE DEVELOPMENTAL SPECIFICATIONS AND THEY SHALL PREVAIL OVER THOSE PUBLISHED IN THE STANDARD SPECIFICATIONS.

010XX.01 DESCRIPTION

This work consists of furnishing, installing, operating, maintaining, moving, and removing night time lighting to illuminate construction work areas for night work when construction activities (vehicles, equipment, or workers) are within 15 feet (4.6 m) of an open lane of traffic. Night work is defined as any work performed between the times of 30 minutes before sunset and 30 minutes after sunrise.

010XX.02 LIGHTING PLAN

The Contractor shall submit a lighting plan to the Engineer for review no later than the Preconstruction Conference. The lighting plan shall show the areas to be illuminated, type and layout of the lighting systems, and calculations of the averaged maintained lighting intensity.

Night work lighting shall be provided in areas where construction equipment or workers are active, and shall extend a minimum of 50 feet (15.2 m) ahead and behind such equipment or workers. The lighting shall provide a minimum intensity of 5 foot candles (54 lux) over the entire work area described above. The lighting shall be positioned to not interfere with or impede traffic in any direction. The lighting shall be positioned to not cause glare for motorists or spillover onto adjacent properties.

Illumination may be accomplished by using a combination of portable floodlights, equipment lights, roadway lights (temporary or existing), or other lighting methods that will provide the required minimum lighting intensity.

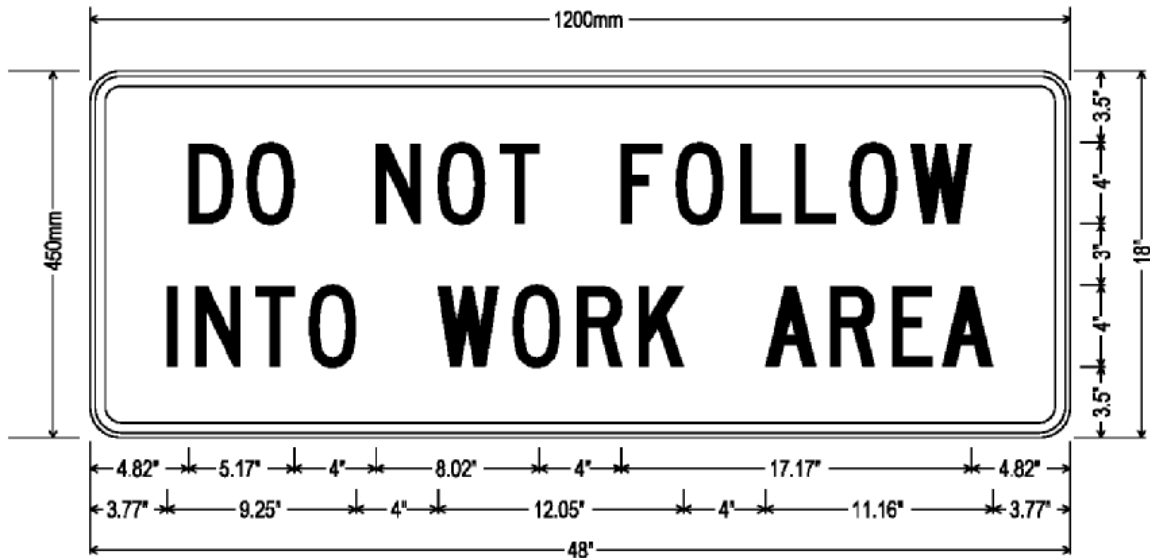
The Engineer may require modifications to the lighting setup in order to fit field conditions.

01XXX.03 LIGHT METER

The Contractor shall furnish to the Engineer one light meter capable of measuring light intensity in foot candles (lux). Instructions for operating the light meter shall be provided to the Engineer. The light meter will remain the property of the Contractor upon completion of night work.

01XXX.04 TRAFFIC CONTROL

All vehicles hauling material to or from the work area shall display an 18 inch by 48 inch (450 mm by 1200 mm) sign with the legend "DO NOT FOLLOW - INTO WORK AREA" as shown below. The sign shall be orange with black lettering using Type VII (Iowa) sheeting. The sign shall be kept clean in order to maintain its visibility.



1.50" Radius, 0.50" Border, 0.25" Indent, Black on Orange;

[DO NOT FOLLOW] C; [INTO WORK AREA] C;

Table of widths and spaces.

4.82"	D	2.19"	O	0.67"	2.31"	4.00"	N	2.19"	0.84"	O	2.32"	0.67"	T	2.00"										
	4.00"	F	2.00"	0.68"	2.31"	0.84"	L	2.00"	0.68"	L	2.00"	0.67"	O	2.32"										
													0.67"	3.00"	4.82"									
3.77"	I	0.56"	0.84"	N	2.19"	0.68"	T	2.00"	0.67"	O	2.31"	4.00"	W	3.00"	0.68"	O	2.31"	0.85"	R	2.18"	0.84"	K	2.19"	
	4.00"	A	2.50"	0.68"	R	2.19"	0.84"	E	2.00"	0.45"	A	2.50"	3.77"											

All Contractor's vehicles and equipment (except for hand operated equipment) operating within 15 feet (4.6 m) of an open lane of traffic and all vehicles and equipment entering or exiting the work area shall display amber high intensity rotating, flashing, or oscillating lights.

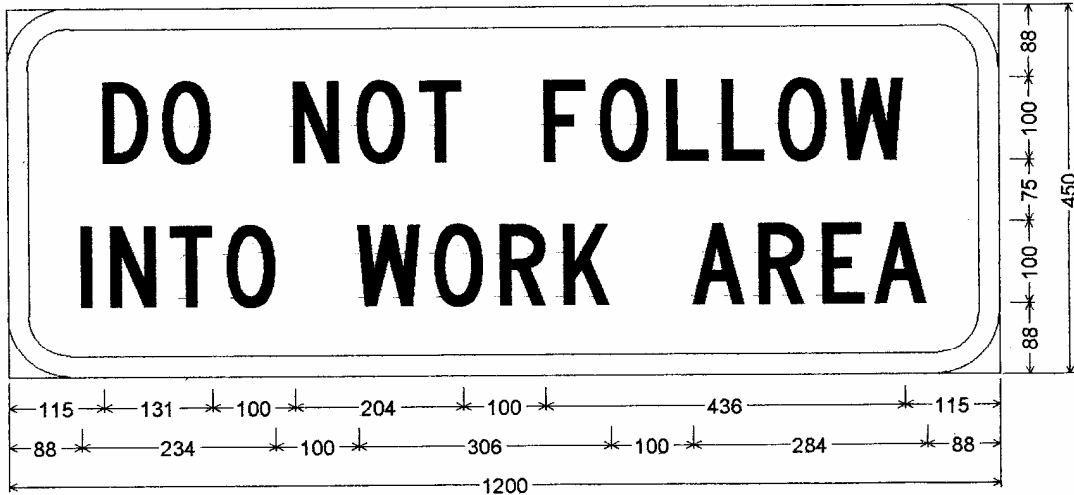
All traffic control devices shall be placed and removed when possible, during daytime hours, unless otherwise specified in the contract documents.

The Contractor shall continually review all traffic control devices, including monitoring of lights, to ensure proper installation and working order.

01XXX.05 METHOD OF MEASUREMENT AND BASIS OF PAYMENT.

All costs associated with furnishing, installing, operating, maintaining, moving, and removing night work lighting and other traffic control requirements required by this Developmental Specification, shall be considered incidental to the lump sum bid price for Traffic Control.

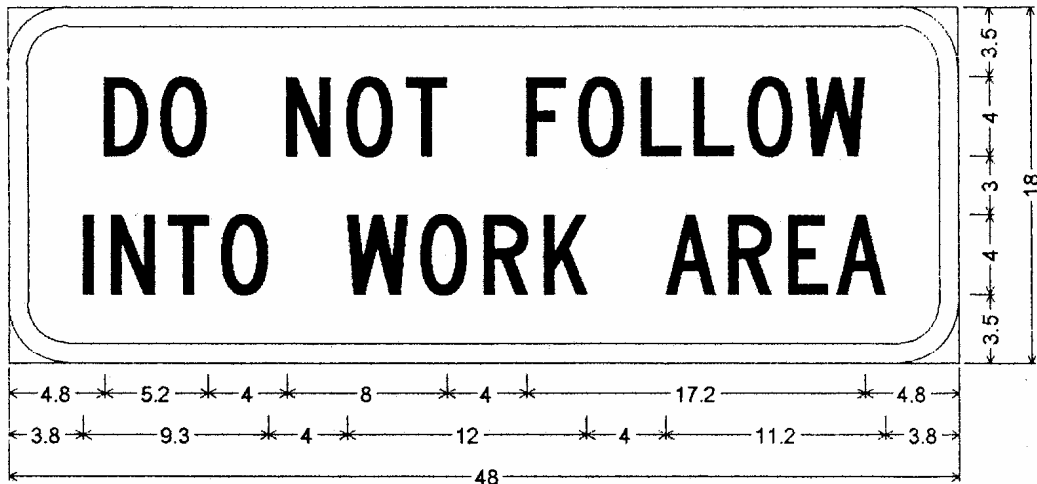
METRIC



75mm Radius, 25mm Border, Black on Orange;
 "DO NOT FOLLOW" C; "INTO WORK AREA" C;
 Table of widths and spaces.

	D	O	N	O	T	F	O	L	L	O	W													
115	56	16	58	101	56	21	58	18	51	100	51	16	58	22	51	18	51	16	58	18	76	115		
	I	N	T	O	W	O	R	K	A	R	E	A												
88	14	22	56	16	51	17	58	101	76	17	58	22	56	21	56	99	64	17	56	21	51	11	64	88

ENGLISH



3.0" Radius, 1.0" Border, Black on Orange;
 "DO NOT FOLLOW" C; "INTO WORK AREA" C;
 Table of widths and spaces.

	D	O	N	O	T	F	O	L	L	O	W													
4.8	2.2	0.7	2.3	4.0	2.2	0.8	2.3	0.7	2.0	4.0	2.0	0.7	2.3	0.8	2.0	0.7	2.0	0.7	2.3	0.7	2.3	0.7	3.0	4.8
	I	N	T	O	W	O	R	K	A	R	E	A												
3.8	0.6	0.8	2.2	0.6	2.0	0.7	2.3	4.0	3.0	0.7	2.3	0.9	2.2	0.8	2.2	4.0	2.5	0.6	2.2	0.9	2.0	0.4	2.5	3.8

Standard Specification Manual Rewrite

Item 6

The Office of Materials explained that they meet once per year with the ILPA. ILPA asked if the Specifications Committee would consider implementing the imperative mood/active voice rewrite for the April 2006 letting. The Office of Contracts asked how this implementation would be done. The Specifications Section noted that it could be done either as part of the General Supplemental Specification or as a Supplemental Specification. The difficulty with implementing, as an SS, is that it must be attached to every project for which it applies. The Specification Section also noted that the rewrite will need to be reviewed by the committee, and that the November meeting is the last meeting to include items for the April 2006 letting. Another possibility would be to put the rewrite on the web and let readers compare versions.

The Office of Construction asked if the rewrite actually changes any of the specifications. The Office of Materials noted that there are a few minor changes, but for the most part all that changed is how the specifications read. An effort was made to keep cross references the same.

The Specifications Section asked if there would be a problem changing section numbers to eliminate some of the gaps in the numbering. The Office of Construction was not comfortable with that idea. The Specifications Section asked about the possibility of a piecemeal implementation. The committee did not see a problem with that. The committee agreed to place the aggregate specification rewrite in the GS. The Office of Materials will send out a copy of the aggregate specification rewrite for committee members to review. The committee will further discuss implementation at the November meeting.



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OCT 04 2005

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September 30, 2005

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Mr. James R. Berger
Director, Office of Materials
Iowa Department of Transportation
800 Lincoln Way
Ames, IA, 50010

Dear Jim:

I sincerely thank you and your staff for attending the Iowa Limestone Producers Association (ILPA) Technical Committee meeting on September 13.

I believe the cooperation between industry and government illustrated by these meetings pays big dividends. The dividends benefit not only aggregate producers and the Department of Transportation, but ultimately Iowa's tax paying public.

During the meeting we reviewed the proposed revisions to aggregate specifications dated 8/25/2005. These are the revisions that reflect the Department's move towards the use of "imperative" language.

Overall, the ILPA Technical Committee was very supportive of the rewrite. They felt the imperative language was more efficient and reduced much of the flipping back and forth previously required to find referenced sections. There were three areas where the Committee felt improvements might be made in the Specifications themselves.

Section 4110 Fine Aggregate for Concrete:

The Committee would like to maintain 4110 as long as 4111 is an option. They opposed the elimination of 4110 and asked that the DOT consider changing the gradation for course aggregate for QMC in place of 4111.

Section 4131 Porous Back fill:

The Committee supported changing the #4 to 0-50 percent passing.

Section 4130 Erosion Stone:

The Committee felt erosion stone should meet the same gradation and quality specification as virgin aggregate.

I hope these comments are helpful.

Sincerely,

Rich White
Executive Director